



Home of the Kenosha County Fair

MODIFIED DIVISION 2021

Open to two-wheel drive American automobiles provided they comply with, and adhere to, specifications as outlined for this Division.

NOTICE ALL EQUIPMENT IS SUBJECT TO THE APPROVAL OF WILMOT RACEWAY OFFICIALS. NO EQUIPMENT WILL BE CONSIDERED AS HAVING BEEN APPROVED BY REASON OF HAVING PASSED THRU INSPECTION. EFFORTS TO TAKE ADVANTAGE OF "LOOP HOLES" IN THESE RULES WILL NOT BE TOLERATED. ALL RACE CARS ARE SUBJECT TO INSPECTION BY TRACK OFFICIALS AT ANY TIME.

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publication of, or compliance with, these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

Numbers are required on the door or quarter panel and must be at least 20" tall and 4" wide. A number is required on the roof, readable from the right side, and must be at least 30" tall. Numbers must also be on the front and back of the car for ease of lineups, and be at least 4" tall. NO METALLIC OR HOLOGRAPHIC NUMBERS WILL BE ALLOWED. Numbers need to be of contrasting color to the main color scheme. If there are double numbers a letter will be issued to you by the track to go with your number. If a letter is required with the car number, the letter must be at least fifty percent (50%) of the height of the number. IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE SCORERS CAN READ THE CAR NUMBER. IF YOUR CAR NUMBER IS NOT READABLE FROM THE SCORING TOWER THE CAR WILL NOT BE SCORED UNTIL THE NUMBER IS CORRECTED.

I. SAFETY EQUIPMENT

A full face, helmet and face shield, Snell-rated SA2010 or SFI 31.1/2005 or newer is required. Helmet and face shield must be worn at all times car is on the track and must accompany vehicle at time of inspection. Roll bar padding required in driver compartment (Fire Retardant recommended). SFI approved full fire suit, fire retardant gloves, and shoes required. Fire retardant neck brace optional. Drivers not wearing neck collar will be required to wear a Fire Retardant head sock. Recommended: SFI Fire retardant head sock and underwear (Mandatory for driver under the age of 18); head and neck restraints; collapsible steering shaft. Driver-side Window net required, minimum 16 "x 20" ribbon or meshes style, and must be mounted to roll cage so latch is at top of window. Maximum four inch tall visor attached to window net. The window net must be up and latched at all times the car is on the track. Minimum three-inch (two inch with head restraint system) wide, SFI approved five-point safety belt assembly required (Y-type shoulder harness not allowed), must be mounted securely to roll cage. Seat belts must be within manufacturer's expiration date. No expired seat belts will be allowed. Kill switch required within easy reach of driver and must be clearly marked "OFF" and "ON",

2. FRAME

OEM complete, 1964 or newer, perimeter American rear-wheel drive passenger car frame only. No sports car frames. No Jeep, Bronco, etc., or four-wheel drive frames. No front-wheel drive frames. Frame must be full and complete, cannot be widened or narrowed, and must be able to support roll cage of both sides, except: weight jack in original center line of spring tower allowed; frame may be cut a maximum 36 inches forward from center of rear end housing; horns may be removed in front of steering box; front cross member may be notched and boxed for radiator and/or steering clearance; maximum seven-inch wide opening in side of spring tower for spring removal. Maximum two-inch wide by four-inch tall frame stiffener may be welded directly to outside of left side frame rail. Left top frame rail can be removed inside cockpit. Minimum wheelbase 108 inches, maximum 112 inches, both sides. Maximum overall width (front or rear) shall not exceed 78 inches from outside of tire to outside of tire. No part of frame or body can be lower than four inches from the ground except front cross member and rear underslung.

3. ROLL CAGE

Must consist of continuous hoops, minimum 1.75- inch O.D. tubing, with; a minimum wall thickness of 0.095-inch for main cage, frame-mounted in at least six places, low carbon or mild steel recommend. No brazing or soldering allowed. Must consist of a configuration of front, rear and top hoops, connected by tubing on sides or side hoops. Driver's head must not protrude outside cage with helmet on. Roll cage must be securely supported and braced with minimum one cross bar in top halo. Foot protection bar required. Main cage no further forward than rear of engine. All bars forward of cage must be lower than hood.

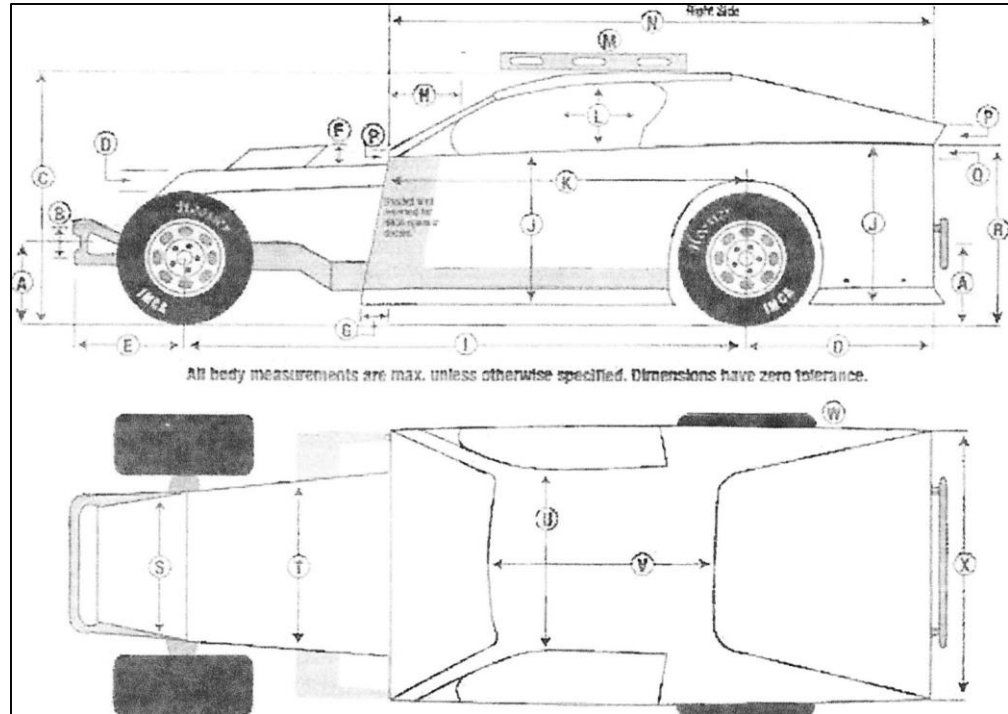
4. DOOR BARS

All driver side door bars and uprights must be minimum 1.50-inch O.D., and 0.083-inch wall thickness. Minimum three driver side door bars, parallel to ground and perpendicular to driver, and welded to front and rear of roll cage. Passenger Side must have at least one cross door bar, horizontal or angled, minimum 1.25-inch O.D. and 0.083-inch wall thickness, and one top horizontal door bar, minimum 1.50-inch O.D. and 0.083-inch wall thickness. Steel door plate, 18 gauge or 0.049-inch minimum thickness, must be securely welded to outside of door bars and cover area from top door bar to bottom door bar and from rear hoop down-post to 5-inches in front of seat. Must be visible for inspection.

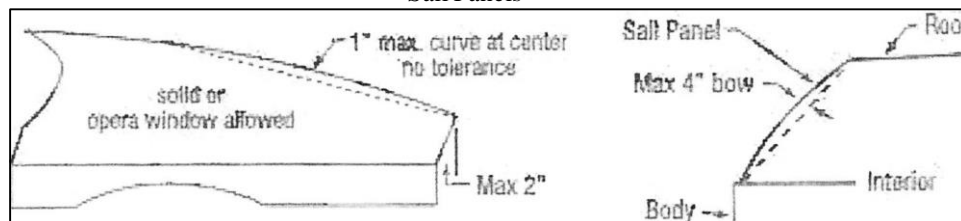
5. BODY

(See diagram) No composite or plastic body panels allowed except-roof rock guard and hood scoop:

Body and interior deck must be same width, front to rear, and parallel to OEM frame. Aluminum nose panel must be flat. Maximum 2.250 inch side fins allowed on aluminum nose. IMCA approved plastic nosepieces allowed. Nose must be mounted in an approved manner and can extend no higher than front top of hood. Nose piece must remain inside confines of front bumper (exception is plastic valance) and be no lower than four inches below frame horns. Cooling holes allowed. Engine compartment must remain open (no side panels). Hood must cover radiator, be level or sloped down at front, enclosed and maximum two inches above interior deck at rear. No reverse hood rake allowed. No panel in front of right door to engine compartment. No inner panels. No car covers. Must have front and rear roof support posts. Driver and passenger side windows must have at least 12 inch opening (height and width,) measured at center of window, between lowest point at top and highest point at bottom. Sail panels may not extend ahead of back of seat. May use Lexan in sail panels. Roof must be fiberglass or aluminum, full size and rounded down in all directions and mounted within ½" above rear of roll cage. No dished roofs allowed. Driver roof hatch allowed. Maximum 1.5 inch rolled down rock guard allowed on roof front. Maximum four inch roof sides allowed. Maximum one inch ridge down sides of roof. Maximum one inch rear roof stiffener (must face down.) No additional fins, lips, wings, spoilers or vortex generators. Maximum four inch plastic skirting allowed on bottom of doors and quarters and nose. No reflective doors or quarter panels. Body may be maximum one inch outside of rear tires (both sides) for clearance. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on sides, top and back of car, and front, if possible.



Sail Panels



Modified

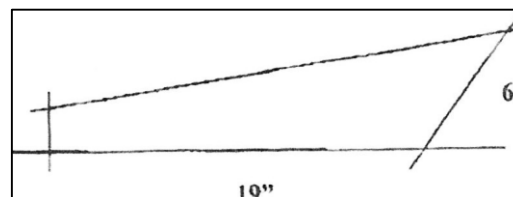
See IMCA rule [look for additional sail panel examples](#)

ALUMINUM OR LEXAN SPOILER OPTION

One (1) inch max 90° curl at top of spoiler allowed.

Max. Width not to exceed width of body height measured on flat of spoiler

Max. 6" Supports and Braces must not exceed these dimensions.



*Must attach right at front of rear quarter panel and decking meet.
Bottom of spoiler cannot be mounted behind furthest point back on quarter panel.

MODIFIED BODY DIMENSIONS

- A. 20" max , 16" min. ground to center of bumper)
- B. 6.5" min. (center to center)
- C. 54"max. cage. 56" max roof. 42" min. both
- D. 6" max. Hood drop (sides), sealed off from driver's compartment and max. 6" rake
- E. 36" max.
- F. 6" max. scoop cannot extend past front of hood
- G. 4" min ground clearance. Door may extend max. 6" past block at bottom on both sides
- H. 19" max. must be same on both sides
- I. 112" max. 108" min.
- J. 31" max. 22" min.
- K. 72" max. or not past back of block, at top Left side may extend forward to cover foot box, if necessary.
- L. 18" max. 12" min. opening, both sides.
- M. With level must have no more than 2" clearance at rear of roof, and 5" at top front
- N. 120" max. 106" min.
- O. 48" max. 34" min.
- P. 4" max. at rear, gradual slope from roof to this point.
- Q. interior slope is 6" max. front to rear and flat across. If flat at front half of interior you have used up 50% of your 6" so from behind driver to rear you may only have 3". Top of interior must be flush with top of door and quarter panels. An optional escape hatch from driver to passenger door to provide 12" opening allowed.
- R. 38" max. 28" min.
- S. Min. 24" nose must be flat and flush with sides.
- T. 66" max. 24" min. no wider than radiator.
- U. 52" max. 42" min.
- V. 56" max. 41" min.
- W. Tires max 1" inside body both sides
- X. 66" max. 53" min. measured at top of interior deck. Must be same front to back. Rear panel - 8" min, 1 90 degree angle. Must be solid, attached to deck. extend to quarter panels, securely fastened.

Sail panels - Must extend from roof to rear of car and be same on both sides. May extend max. 2" past rear edge of interior deck. Must mount within 2" of outside edge of interior deck.

6. DRIVER COMPARTMENT

Must have minimum three windshield bars in front of driver. Lexan or aluminum cowl panel in front of driver can be no wider than cockpit and no farther back than steering wheel. Minimum 0.125-inch aluminum or 0.060-inch steel, complete floor pan required. Aluminum high-back seat only and must be bolted in using minimum of four 0.375-inch bolts, next to left side frame rail and ahead of rear tires. Bottom of seat can be no lower than bottom of frame rail. Driver must be sealed off from track, driveline, engine, fuel cell, canisters and pumps. No equipment shall protrude over decking. Oil accumulators (Accusump's) may not be located in the driver's compartment. No devices that would enable driver adjustment to alter wheelbase or for weight jacking while car is in competition. No mirrors or reflective sun visors.

7. FRONT SUSPENSION

All components must be steel, unaltered OEM, in OEM location, and replaceable by OEM parts, except: tube-type upper A-frames with or without aluminum/steel cross shaft, and mounts can be moved; stamped steel OEM replacement lower A-frames; rubber, nylon or steel lower A-frame bushings, no offset or bearing type; welded or bolted shock mount on lower A-frame. Lower A-frames must be right and left, and of same design. Lower A-frame mounts and bolt holes on frame must be in OEM specifications. OEM or OEM replacement ball joints only. Sway bar must be unaltered OEM. No suspension stops of any kind allowed.

8. STEERING

No rack and pinion. All components must be steel, unaltered OEM, in OEM location. Exceptions are: outer tie rod end and adjustment sleeve may be replaced by a minimum 0.625 inch steel rod end and steel tube; spindles can be ground for brake caliper clearance only; unaltered, OEM or OEM replacement Pinto spindles with 'IMCA' raised cast; replacement spindle with Speedway Motors raised cast - part numbers 91034501-1- and 91034501 -R; bolt on spindle savers allowed; Speedway motors IMCA approved multipiece spindles allowed. Steel steering shafts and knuckles only; driver compartment steering may be modified, must be kept on left side. Spindles must be right and left, and of same design. Quick release required — steering quickener and steering wheel may be aluminum. Idler arm, pitman arm, and center link must match frame.

9. SHOCKS

One steel, nonadjustable, unaltered shock per wheel. All shocks must completely extend & collapse at any time. One additional shock allowed in pull-bar area. No external or internal bumpers or stops. No threaded body, front coil -over, air or

remote reservoir shocks. No Schrader or bladder type valve allowed. Front half may be shielded. Maximum 7" stroke front shocks maximum 9" stroke rear shocks.

10. SPRINGS

One steel, non-progressive, coil spring per wheel. Steel or composite leaf spring allowed. One additional spring allowed on pull bar or lift bar, may be progressive. Any coil spring must be at least 4.5-inches O.D. No torsion bars or air bags.

11. REAR SUSPENSION:

Rear of frame may be altered to accept leaf or coil springs. All components must be steel. All trailing arms/link bars must be solid tubing. One mechanical traction pull bar allowed. Rubber bumpers allowed on pull bar or pan hard bar only. Minimum 19 inch long panhard bar measured straight center to center. One bracket mounted solid to axle tube with lower link OR one floating birdcage with upper and lower links allowed per side. Additional shock/coil-over eliminator clamp bracket solidly mounted to axle tube is allowed per side. Steel coil-over eliminators, or steel or aluminum coil-over kits allowed - must conform to shock and spring rules. Shocks and coil-over eliminators must be mounted to birdcage or bracket below bottom of axle tube and to upper frame rail. Spring using jack bolt may be mounted directly to top of axle housing. Solid safety chains securely mounted from upper frame rail directly to axle tube allowed (cannot be mounted to any floating device) Chain must be slack during inspection. No independent rear suspension. No covers on any suspension components. No lift, brake or sway bars. No suspension stops or adjustable underslung of any kind.

12. REAR END

Any steel approved OEM passenger car or truck rear end allowed. Safety hubs (floater) allowed. All components must be steel, except lowering blocks, axle cap, and driver flange. Inspection hole in housing is required. Full steel mini spool or welded rear ends only. Steel axles only. Gun drilled axles are permitted. Standard weight Steel tube quick change allowed, quick change must use 10" ring gear with aluminum or steel spool. Quick change must use minimum one inch wide spur gears & bolt rear cover. One-piece drive flange only. No torque dividing differentials. No scalloped or lightened ring gears, no cambered rear ends.

13. BUMPER/RUB RAILS

Steel only. Bumpers must be used both front and rear at all times and welded, or mounted with minimum 0.375-incl. bolts. Rear bumper may be a maximum of six inches beyond rear deck. Must be minimum 1.25-inch tubing, 0.065 wall thickness on front — 0.095 wall thickness on rear. Two-bar front bumpers must be mounted frame-end to frame-end, no wider than width of material outside frame horns and with bottom loop parallel to ground. Top bar must be directly above bottom bar, minimum

6.5-inches apart, measured center to center. Rear bumper and nerf bars can be no wider than tires.

14. TIRES/WHEELS

Must use unaltered Hoosier tire G-60•15 with I.M.C.A. stamped on sidewall. No softening or grooving of tires. Tire grinding or straight siping permitted on the tread face only of the tire. May not extend past factory edge line of tire. Tire reconditioning subject to approval of technical inspector. No recaps. All wheels must be steel, 15inches diameter, 8-inches wide, minimum shell thickness 0.095-inches, and minimum 19.0 pounds. No wide five wheel adapters allowed. Aluminum spacers only. May use external steel bead lock on right rear only and it cannot make wheel any narrower than 8-inches and no wider than 8.75-inches. Must use only steel bolts. Foam type or soft plastic mud plug allowed on right side tires only. All wheel covers must be mounted with steel threaded fasteners. All mud_cover mounting tabs must be integral part of the wheel. May use an inner mud plug on the left rear only. Rim- mounted bleeder valves allowed. A MINIMUM TIRE PRESSURE OF 12 LBS WILL BE ENFORCED ON THE RIGHT REAR.

15. BRAKES

Must be steel approved OEM, operative four-wheel drum or disc. Must maintain minimum OEM dimensions for hubs/rotors and calipers cannot be lightened. Bolt pattern may be changed. Larger studs allowed. Rear rotors may be aftermarket, minimum 0.810-inch thickness. Vented rotors only, no scalloped rotors. No brake shut-off or pressure sensitive devices. One proportioning device allowed, front to rear only. Brake lines must be visible. Single link brake caliper floater allowed on rear, one per side.

16. EXHAUST

Round tube headers only. All primary header tubes must enter directly into one collector, at same point, at end of header. Collector length maximum 9" long. Turn downs permitted, maximum 10" long. Mufflers mandatory at all times car is on the track. Loss of muffler is an automatic mechanical disqualification. 95-decibel limits 100 feet from car. All exhaust must go through mufflers, two per car, and one per header. No merge collectors, extensions, inserts, cones or balance tubes. No exhaust sensors. Pan evac systems allowed.

17. FUEL SYSTEM

Mechanical or belt driven fuel pump only and must be mounted at front of engine. Racing fuel cell required, maximum 32-gallon capacity must be in minimum 20-gauge steel container. Cell must be securely mounted behind rear axle, between rear tires, minimum of four inches ahead of bumper, minimum of ten inches above ground. Must mount with minimum one-inch square tubing or two 0.125-inch thick solid steel straps, two inches wide around entire cell. All cell mounts must be steel, securely welded to frame/cage. Protective tubing must cover rear and extend past both sides of cell. No part of cell shall be lower than protective tubing. Fuel cell vents, including cap vent, must have check valves. Pick-up must not be underneath fuel cell. Limit of one fuel

filter. No cool cans. Conventional air cleaner housings only. Air cleaner top/stud cannot direct air into carburetor. No top flow air cleaner housings. One naturally aspirated 2- or 4-barrel carburetor only. No adjustable carb spacers.

18. FUEL

Gasoline, E85, E98 or alcohol. NO nitrous oxide. NO performance-enhancing additives. Upper cylinder lube allowed. Fuel sample may be taken from any car at any time. Penalty for illegal fuel is disqualification from event and \$250 fine - first offense.

19. WEIGHT

Minimum weight limit of 2,450 pounds, no tolerance, after race with driver in car. No weights and/or loose objects in driver compartment or outside body. Weights must be securely mounted to frame or roll cage and painted white with car number on it. Must be attached with at least two 0.5-inch bolts. No titanium, magnesium or carbon fiber or tungsten products. No gun-drilled, tubular, hollow bolts or studs. Steel fasteners only.

20. BATTERY/STARTER

One 12-volt battery only, must be shielded and securely mounted between frame rails, and positive terminal covered. Must have capability of starting without being pushed or pulled. Must leave initial staging area on demand, unaided, or go to rear of that race. Reverse-mount starters with OEM case transmissions only, see transmission rules for specifics.

21. GAUGES/ELECTRONICS

No transmitting or listening devices, timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except analog tach. No adjustable ignition control boxes. Only one 12-volt ignition box allowed. Must be out of driver's reach. No additional ignition accessories allowed. Only change allowed to ignition box is one high-end rev-limiter setting. This setting can be changed through one chip only, or an internal setting inside box. No magnetos. No electronic traction control devices. No electronic advance curve ignitions allowed.

22. TRANSMISSION/DRIVE SHAFT

Must have at least one forward gears and one reverse, plus a neutral position. With engine running and car in still position, must be able to engage car in gear and move forward then backward. OEM production type or approved after-market manual transmissions allowed - one-speed, two-speed, three-speed, four-speed and automatic. No five speed (or more) transmissions, 'in and out' boxes, or quick change devices allowed. Functioning shift levers must be in OEM location on all OEM production type transmissions. All belt drive pumps must be mounted on front of engine. Flex plates must be full, steel, unaltered OEM or OEM replacement. Flywheel/flex plate must bolt

to engine between clutch assembly and crankshaft and all driveline components within bellhousing must rotate while car is in any gear. Transmissions must be one of the following designs:

OEM Case Manual Transmission

Must have a standard OEM case and working disc-type clutch, or approved cone or disc-type coupler inside an explosion-proof steel bellhousing. One flywheel only, minimum 8.5-inch diameter. Diameter of clutch disc must be a minimum of 5.5-inches. Clutch assembly must be steel, except housing, which must be steel and/or aluminum. Bellhousing can have only a hole for throw out bearing lever or hose, must be 270- degrees around top of clutch and flywheel area Standard or reverse mount starter allowed. must directly engage flywheel.

Automatic Transmission

Must remain in OEM or OEM replacement case, with a functioning OEM appearing pump. Must have approved scatter shield or blanket. Scatter shield must be constructed of minimum of 0.25-inch by three inch steel 270-degrees around flex plate.

Aftermarket Transmission

Must have aluminum case, with internal clutch. Internal clutches made of other materials (e.g. magnesium, composites, carbon fiber, etc.) are prohibited. Must bolt to steel, or aluminum bellhousing. Must have approved scatter shield or blanket. Scatter shield must be constructed of minimum of 0.25-inch by three-inch steel 270-degrees around flex plate. Must use full, steel, unaltered OEM or OEM replacement flex plate with stock type starter mounted in OEM location. Lightweight flex plates are prohibited. No coatings or paint allowed on transmission case. No ball spline transmissions.

Drive Shaft

Minimum two-inch diameter steel drive shaft and must be painted white. Steel slip-yokes only. Drive shaft must be painted white. 360-degree drive shaft loop required and must be constructed of at least 0.25 inch by two-inch steel, or one-inch tubing, mounted six inches back from front U-joint.

23. ENGINE COMPARTMENT

Rear of engine (bellhousing flange) must be mounted at least 72-inches forward from centerline of rear axle. Engine offset must be kept within 2 inches of centerline of front cross member with engine level.

Minimum 1 1/2 -inch engine height from ground to center of crankshaft. Radiator must be mounted in front of engine. Cooling system may be modified. Overflow tubes must be directed to ground between frame rails. Use of antifreeze is prohibited. Water wetter is permitted.

24. ENGINE SPECIFICATIONS

Any Ford, GM or Chrysler engine allowed. Steel heads, block and oil pan only. Aftermarket and performance steel blocks allowed. No magnetos or crank triggers allowed. All engines must be able to be used in conventional passenger car without alterations. Engine mounts cannot be removed or altered, castings and fittings must not be changed. No machine work outside of engine. All belt, chain, gear, and direct driven accessories must be on front of engine. Wet oiling systems only. Flat tappet or roller cam/lifters, stud mounted rocker arms only. Cannot alter lifter bores, OEM firing order cannot be changed. GM crate engine permitted.. Crate must use unaltered factory sealed GM#88958604 or #19318604 engine. Engine must be inspected and approved by Wilmot Raceway officials. Additional seals may be added by officials. Altered or missing seals will result in immediate disqualification, suspension & fine to be determined by Wilmot Raceway officials. Crate engine users must display crate on the A - pillar. Mandatory valve train inspection to any crate motor finishing in top four of main event.

25. ENGINE CLAIM. SWAP RULES

None

26. EIRI (Except in rare Instances)

Decisions of Official(s) are final and binding without exception. In some cases, track safety rules may take precedence over rules - any discrepancy between track rules should be brought to the attention of officials at Wilmot Raceway. Any rule changes or clarifications during the course of the year will be published and will be considered as an official part of these rules.

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They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator, or official. Interpretation or amendments to these rules may be made at any time. The rules and or regulations set forth herein are designed to provide orderly conduct and to establish minimum requirements for the racing events. All participants are deemed to have complied with these rules upon participating in these events. No expressed or implied warranty of safety shall result from publication or compliance with these rules or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to participants. The Modified Technical Inspector shall be empowered to permit minor deviations from any specifications herein or to impose

any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from alterations of specifications. All interpretations or deviations of these rules are left to the Wilmot Raceway Officials. THEIR DECISION IS FINAL!

Effective Date: January 9, 2021